

BIG INCREASE IN NUMBER OF AUTOS

Business of Automobile
Bureau Far Outstrips
Last Year.

LOW POWERED CARS
ARE MOST POPULAR

Official Figures Also Disclose Big
Growth in Number of Com-
mercial Vehicles.

Nearly 34 per cent increase over last year in the amount of business was transacted by the Automobile Bureau of New York State during the last ten months, according to the figures tabulated under the direction of Mitchell May, Secretary of State.

The total net receipts from fees for licenses and registration certificates were \$1,250,000, as compared with \$1,060,000 in 1912 and \$850,000 in 1911. There were registered during this year up to December 12 123,000 owners, 57,000 chauffeurs and 1,800 dealers, as against 106,000 owners, 45,000 chauffeurs and 1,700 dealers in 1912. In 1911 there were 82,000 owners, 35,000 chauffeurs and 1,300 dealers.

In Massachusetts the official figures show a gain of 15 per cent in the amount of business transacted over that of last year, when \$616,245 was collected from fees, as against \$564,000 in the present year, 62,963 machines being registered this year and 50,132 in 1912.

The increased use of commercial cars is one of the most notable features. A gain of upward of 61 per cent is shown over last year, more than 13,000, as against 8,500 being registered, while Massachusetts records an increase of 47 per cent, or 5,948.

The figures also spread interesting light on the types of motor cars most in common use. Approximately 60,000 cars of 25 horsepower or less were registered, as compared with 28,000 in 1912. There were 40,000 cars registered under 35 horsepower, against 24,000 in 1912, and 17,000 machines of less than 50 horsepower, to 13,000 registered in 1912. A gain of only twenty cars is shown in the number of machines of 50 horsepower or more, increasing from 1,300 to 1,320.

The records in addition show that the number of new cars originally registered after August 1 reached approximately 10,000, as compared with 8,500 in 1912, and while this total reveals only a slight increase over last year, the percentage of gain in the number of these new machines of twenty-five horsepower or less seems to be higher, reaching 1,600. There were 140 more machines under thirty-five horsepower, 200 more under fifty horsepower, and exactly 31 machines less over fifty horsepower, only 64 cars, against 95 in 1912.

In the county registration figures, New York still leads, with approximately 23,000 pleasure and 6,000 commercial, Kings following with less than one-half of this number of pleasure cars and one-half in commercial. Erie County is next with nearly 9,000 pleasure and 1,500 commercial. Westchester has recorded 8,000 pleasure and 600 commercial, while Monroe shows a slight decrease in pleasure, with a correspondingly large increase in commercial, the total of these five counties amounting to 55,000 pleasure and 10,000 commercial.

The tremendous growth in the use of motor vehicles in this state is demonstrated by the registration and license figures, together with the total receipts for the last twelve years. In the early years, however, annual registration was unnecessary, so that the figures up to 1910 do not show the total number of machines in operation; only those registered in each year. When the present law took effect in August, 1910, there was a re-registration of all machines.

BUILDS TIRE WITH DETACHABLE TREAD

Great Advantages Claimed for
Goodyear's Latest
Idea.

The Goodyear Company has brought out a new type of pneumatic automobile tire that possesses new features of great interest. It consists of a regular tire carcass and a detachable tread. When the tire is deflated the tread can be removed easily, but when properly inflated the two parts possess the strength and power of a one-piece tire.

This makes it possible to replace either part in a few moments, anywhere, and means that practically a new tire can be obtained, if needed, at about half the usual cost.

The tread is held firmly to the carcass by the inflation of the tire. A secret construction that gives the edges of the tread an unbreakable bond solves the problem of the necessary grip, and keeps the components safe from invasion by water or dirt. It is worthy of note that no mechanical fastenings of any kind are necessary. This means there is nothing to impair the resiliency of the tire, or to add extra weight.

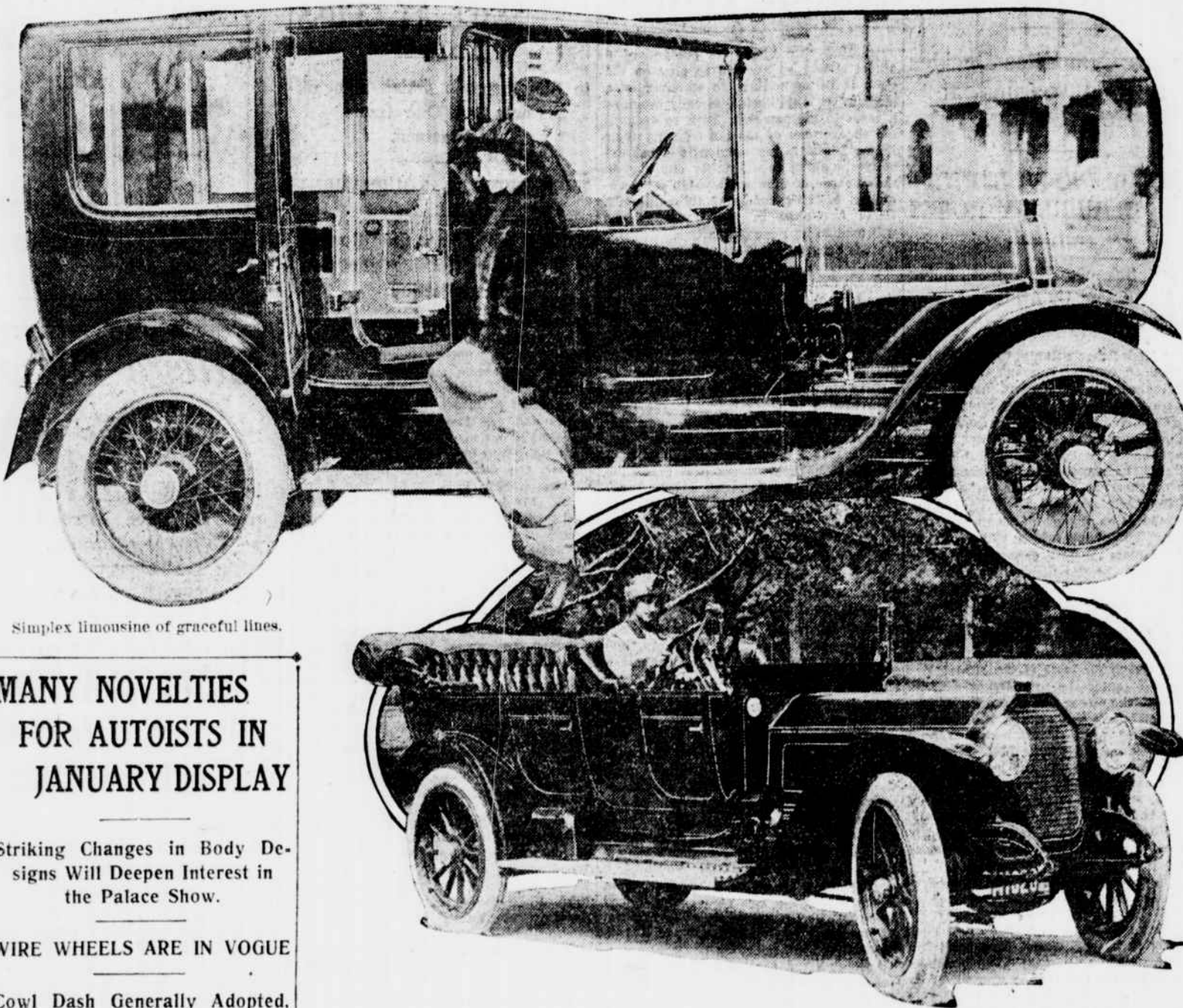
The new tire gives unusual protection from punctures. The tread doesn't bear the tube strain and is therefore less tense than the carcass. Tests prove that a sharp object cannot readily pass through a soft medium into a hard one without being deflected.

The two parts of the tire are built separately and to exact measurements, and when put together they are a perfect fit. There is no loss of power and no danger of the tread being forced off on a curve. The wearing surface is of sharp-edged, wide-based blocks, which assure protection against skidding. The Akron Company is confident that the two-piece tire will soon occupy a front rank.

Automobile Route Recognized.

The honor of being the first city in the United States on the route of the Lincoln Highway to officially change the name of its principal thoroughfare, Main street, to Lincoln Highway, is claimed by De Kalb, Ill. This action was taken at a special meeting of the City Council a few days ago. The highway runs east and west through Illinois, and passes through the business section of De Kalb. From New York to San Francisco propositions similarly recognizing the route, dropping the old designations, are now being considered by many cities and towns that do not already possess a thoroughfare named after the great Emancipator.

NEW TYPES OF AUTOMOBILES, WHICH WILL BE SEEN AT SHOW IN GRAND CENTRAL PALACE



Simplex limousine of graceful lines.

MANY NOVELTIES FOR AUTOISTS IN JANUARY DISPLAY

Striking Changes in Body De-
signs Will Deepen Interest in
the Palace Show.

WIRE WHEELS ARE IN VOGUE

Cowl Dash Generally Adopted,
and Controls Are Centred
Near Driver's Seat.

It is true that recent improvements in cars from year to year have not been quite as radical as some years ago, owing to the fact that the industry has become standardized to a large extent. However, many of the changes which have been made in car construction since last season are important ones—points that the prospective purchaser of an automobile should know about.

One of the most striking new features to be noticed generally in the show which opens on January 3 in Grand Central Palace is wire wheel equipment. Producers of the new type of wheel claim for it many advantageous qualities. Electric lighting of automobiles by dynamo systems has ceased to be a novelty, but the new crop brings a number of refinements. Many changes have been made to eliminate difficulties entailed by short circuiting and other freaks of electric power. This is also true of the electric self-starters put out by various companies. Incidentally, this season sees many more makes of electric self-starters than previous seasons. Much improvement has been made in this line.

Makers generally are devoting more attention to body design than formerly. There are more coupe types, and some of the body makers are designing inside drive vehicles, with novel seating arrangements. Some are putting out coupe bodies which seat three and even four comfortably.

More attention is being paid to the deepening of the cowl dash, the narrowing of the windshield bringing it nearer to the front seat than heretofore without shortening the foot room. This means far more comfortable riding on a windy day. Switches for the lighting system, self-starter, etc., are neatly taken care of under the cowl, while some models have all the switches concealed in a single control box, clamped to the steering post directly beneath the steering wheel.

Numerous new models have sloping hoods. Cars designed in this fashion are more like the European product. Extremely deep upholstery will be found on the majority of makes exhibited.

Convertible bodies are proving their worth for the owner who likes an open car ordinarily and a closed body for inclement weather. Several makers are equipping with a body, the top of which can be raised or lowered, and glass sides in panels also raised to make a closed car. Still more makers are adopting the left drive with centrally located control. Several have adopted the electrically controlled gear shift, doing away with side levers entirely. The left hand drive is convenient in many respects, as it permits easy entrance and exit to the driver's seat when the car pulls up at a curb.

Numerous new forms of shock absorbers or supplementary springs have appeared on the market. The popularity of the coil spring type, attached to the rear spring-set of the car, has gained remarkably, while there are numerous designs of hydraulic and friction types of shock preventer. One maker has brought out a shock absorber which consists of an inverted set of spring leaves, shorter than the regular spring set and clamped to this main set with a spacing block between. Shock absorbers now come as standard equipment on many makes, whereas a few seasons back hardly any cars were delivered to the user with these accessories.

A couple of makers are bringing out two-speed axles. This type is particularly advantageous when frequent gear shift is necessary, as, for example, in crowded traffic and very hilly country. With the two-speed axle most of the driving is direct and not through gears, which means less vibration, less noise and less wear.

NEW GAME IN THE WATER

Form of Basketball Being Tried
by Swimmers in Chicago.

The University of Chicago swimmers, under the direction of White, the coach, have been trying out the new aquatic game known as water basketball in order to determine whether the game will prove a practicable substitute for water polo.

The sport was officially abolished by a conference ruling two years ago, and since that time efforts have been made to secure a water contest which will prove an equal attraction. Water polo was generally regarded as the roughest intercollegiate sport and has been viewed with much disapproval for years.

Peerless "Six" which is said to be ideal for woman drivers.

Automobile Salon Has Biggest List on Record

Thirteen Makes of Cars To Be
Shown in Exhibition at
Hotel Astor.

The largest number of exhibits ever participating in a foreign automobile show in this country will display cars at the Importers' Automobile Salon, which opens in the grand ballroom of the Hotel Astor on January 2, for an eight-day run. The list includes thirteen different makes of cars, two body builders, five makes of tires and two other accessory exhibitors. Altogether seven different nations are represented.

The various makes of foreign cars which will be shown at the Salon are in alphabetical order, as follows: Benz, Bugatti, De Dion-Bouton, Delaunay-Belleville, Fiat, Isotta-Fraschini, Lancia, Marshall-Atter, Mercedes, Minerva, Peugeot, S. G. V. and Simplex. The nations represented by these cars are France, Germany, Italy, Belgium, England and America. There is a possibility of Spain also being represented for the first time in this country, by a Hispano-Suiza, for which an agency is about to be established in New York.

The exhibits of foreign tires and accessories are a new feature for the Salon, but those importing them were able to prove their right to participate. The tire

exhibits include Dunlop, Englebert, Faure, Gaudin and Prowdick, the last named a Russian product. In addition, Dunlop wire wheels will be shown and A. J. Pickard will have a complete line of foreign automobile accessories.

Not only will the Salon have a greater number of exhibitors than any of its predecessors, but the various exhibits will be more complete than ever before. This is due to the fact that the new tariff, which reduced the duty on foreign cars and tires, also provided that these could be brought in for exhibition purposes for a period of six months without the payment of any duty.

Previous to this year the importers were obliged to pay a 45 per cent duty on all cars which they brought over for the annual Salons, and naturally they limited their exhibits to such models as had a ready sale in this country. With only the freight to pay now, nearly all of them have arranged to bring over the complete exhibits of their cars, as they were shown at the recent Paris Salon and London Olympia show.

No attempt will be made to add to the beautiful decorations of the Hotel Astor ballroom. The 1914 Salon will be just as much of a "drawing room" affair as its predecessors, and everything necessary will be done to create the atmosphere of social exclusiveness which so much appeals to the class of people interested in foreign cars.

Chicago Man to Start Three Men in Big Race

E. C. Patterson Will Retain Pi-
lette, Salzer and Lauten-
schlager for Speedway
Race Memorial Day.

E. C. Patterson, the noted Chicago sportsman, who last year backed a Mercedes, with Theodore Pilette at the wheel, in the Indianapolis 500-mile race, plans next year to send a three-cornered team of Europe's best after the \$50,000 prize. Pilette, Salzer and Lautenschlager, in the fliers with which they finished third, fourth and sixth, respectively, in the Grand Prix de Le Mans, trailing only the Delage entries, are his choice.

One car is too uncertain a proposition, Mr. Patterson says. His drivers are among the stars of Europe. Pilette is well known in America because of his performance in the last Indianapolis contest. With the smallest car in the field, only 25 cubic inches piston displacement, he landed an easy fifth. In later European contests, as at Le Mans, he also distinguished himself, finishing well up toward the top. Salzer is least known of the trio. His position as factory engineer and designer would seem to furnish ample proof of his ability, however.

Lautenschlager is the oldest as well as the most picturesque figure of the trio. Before the others had begun driving he had already won a Grand Prix, back in 1908. His last appearance, which was at Le Mans, demonstrated that he had lost none of his former nerve and skill. Extra heavy and tough springs made his ride a difficult one. Both bonnet and spare wheel were jarred off. The finish found his hands blistered from wrist to elbow, yet he refused attention. "Macht nichts aus," was all he would say.

Jules Goux and Georges Boillot, of the Peugeot racing camp, generally conceded to be the world's premier speed men, the former by his victory in the last Indianapolis race, the latter by his French Grand Prix triumphs in 1912 and 1913, are soon to visit the United States, in order to renew acquaintances and to get a line on the 1914 American racing season, it is reported.

Johnny Aitken, of the National factory, who served as Goux's pit man and directed the latter's race during the Hoosier classic, is authority for the statement. The Frenchmen expect to stay a couple of weeks at least. A determined effort

will be made by the Peugeot crew, it is thought, to repeat its performance of last year, when it won the speedway contest hands down.

SPEED KING COOPER WILL RACE AGAIN

Auto Driver, with a Matchless
Record, Is Recovering
from Injuries.

Karl Cooper, without a peer as a road race driver for the season of 1913, is well on the way to recovery from the injuries received at San José, Cal., Saturday, November 24. A cut in the face and two broken bones in a hand are the extent of his hurts, and his skill at the wheel will be in no way impaired.

One of the most prominent of the motor trade journals in America hails Cooper as the greatest of the road racing stars of the season and cites his record of five firsts and one second in six starts as the reason for awarding the laurels. Throughout the year's campaign Cooper's car never was shot with any other than Firestone tires.

The Californian's list of victories is as follows: Broke 200-mile record in Fresno, February 10; second in San Diego road race, March 1; drove relief in Indianapolis races Memorial Day; won Golden Potlatch at Tacoma, July 5; Montanara Fests races at Tacoma, July 7; Santa Monica road races, August 9; first in Corona 650-class, September 9; first in Corona (free-for-all) at Bakersfield, September 28, and first in both fifteen and twenty-five mile races at Fresno, October 4.

Cooper won his first victory in 1912, coming in first at the San José races. In 1915 he was first in the Seattle meet, and he captured the three races of the two-day meet at Tanforan in 1908. After that Cooper's victories were annual affairs. Ten triumphs were placed to his credit in the years 1909, 1910, 1911, 1912.

Rushing New Willys Car.

John N. Willys, president of the Willys-Overland Company, of Toledo, announces that he will have four and six cylinder models of his new Knight-motored car ready for the national automobile shows at New York and Chicago. He has also announced that the new line will be known as the Willys-Knight cars. Work at Mr. Willys's Elkhart plant, where the new car will be manufactured, is being hurried so that production can be got under way as soon as possible.

Arrests for Speeding Hurting Auto Business

Motoring Organization Will Wage
Fight to Better Existing
Conditions.

PERSECUTION IS ALLEGED

No Necessity, It Is Claimed, for
Great Activity Shown on
Recent Sundays.

Rapid progress is being made in the campaign which the Manhattan Automobile Club is conducting for the purpose of lessening public resentment against motor cars and motorists. That the attitude against motorists is considered unfair is best evidenced by the number of letters received at the club's headquarters. By means of the agitation it is hoped to safeguard the reasonable and law-abiding motorist against persecution and to better existing conditions. Interest in the propaganda has already largely increased the membership of the club.

"The object of the crusade is mainly to remove the difficulties thrown into the path of the man who uses the automobile," says E. E. Schwarzkopf, vice-president of the club. "Among them are wholesale arrests through occasional enforcement of the laws, reckless or thrilling chauffeurs, dishonest garages and many other abuses which are enumerated in a pamphlet on 'Protection and Correction,' which we are distributing."

Take, for example, the Sunday proposition in New York of policemen on motorcycles and bicycles and making more than three hundred arrests, all in a single day. During the week only a few arrests are made, and on Sunday motorists are arrested by the wholesale, and in nearly every case fined.

"Whether or not motorists drive recklessly is not taken into consideration. The fact that they are violating the speed limit set by a city ordinance that is absolutely inadequate to take care of the conditions now existing is all that is considered. Things have reached a point where a number of persons who own automobiles have stopped using them and have placed them in dead storage, and prospective buyers are afraid to invest in automobiles. It is absurd to assume that the proportion of speeding cases should be so vastly greater on Sundays than on week days."

"Reckless driving must be curbed, regardless of existing speed limits. Not only will the public benefit through removing these obnoxious practices, but the market for automobiles will be enlarged, and this will benefit automobile builders and manufacturers."

"The Manhattan Automobile Club intends to demonstrate to automobile owners of New York that it can protect their investment in automobiles as it has never before been protected—that it can bring about the removal of many of the nuisances which beset motorists in the streets and garages. It wants to show the public that it can offer protection against the perils and nuisances of reckless driving. It will take stringent measures to secure the right kind of legislation and prevent the wrong kind. Another big thing will be the proper regulation by automobilists themselves in conjunction with the municipal authorities of what has become in the eyes of the public a great problem—automobile traffic in our streets."

KING EMPLOYEES HAPPY

Each Receives One-tenth of
Year's Salary as a Gift.

Detroit, Dec. 20.—The employees of the King Motor Car Company were given a pleasant surprise in the form of a Christmas gift to-day, each one receiving a check amounting to one-tenth of his salary as his share in this year's profits. This gift is in keeping with the ideas of the officers and stockholders, who believe that all engaged in the manufacture of the company's cars should share in its success.

At noon the employees were assembled in one of the large rooms of the factory, where one of the officers made the presentation, announcing that the concern had passed through an unusually successful year, is now shipping ten cars a day, and has in rapid development one of the surprises of the year. Each employee received an envelope containing his particular check, after which luncheon was served.

This action is the first in the history of the automobile industry in Detroit where the employees have shared in the profits, and is the outgrowth of modern business methods.

ENGLISH STROKE TABOO

New Coaches of Yale Crew Re-
organizing Rowing System.

New Haven, Conn., Dec. 20.—In presenting to Yale's alumni body the plans for reorganization of the rowing system "The Alumni Weekly" to-day follows closely the announcement previously made to the undergraduate body, although it does not state that matters are finally settled.

It points out that Richard Armstrong, '96 S., has been made not only graduate manager of the crew, but head coach, while invitations have been extended to Guy Nickalls, coach of the Leander Boat Club, and C. F. Giannini, coach of the New York Athletic Club, to assist him. "The Weekly" says that the plan adopted is a return "to the fundamental conception of college rowing that was the keystone of Yale's long periods of former success on the water."

In relation to a future Yale stroke, the point is made that Mr. Nickalls, while here, told the crew squad that the "English" stroke would be tabooed under his administration. Mr. Nickalls and Mr. Giannini, it is pointed out, agree with Mr. Armstrong in an American-adapted technical stroke—a shorter one than Yale has been rowing in the last two years, and one that will be fitted to the material in the boat. In summing up the outlook "The Weekly" says the occasion is one for much hopefulness to Yale graduates and for "graduate adoption of an attitude of co-operation and submergence of individual notions to the common good."

Howard Yale Hockey Coach.

New Haven, Dec. 1.—Captain Walter Heron of the Yale hockey team announced here to-night that Tom Howard, who coached the hockey team three years ago, would be this year's coach.

AUTOMOBILES.

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SAFETY FIRST

Five Bars and a Cross Tie—
the Safety First Symbol

Five thick, tough Goodrich rubber fingers—
They dig down through mud, slush and ooze; they
clean and grip the road. They stop the skid before it starts.
They give you lower-cost mileage because of the extra thickness
of rubber at the point of contact.

Goodrich Safety Tires

Best in the Long Run—Best in the Short Stop

Don't experiment. Don't take chances. Look for the five bars and the cross-tie when you choose a non-skid tire.
Read the reduced prices below. Better still, tear out this price list and hand it to your dealer.

Don't pay more than the prices named here for the accepted standard non-skid tires:

Size	Smooth Tread Prices	Safety Tread Prices	Size	Smooth Tread Prices	Safety Tread Prices
30 x 3	\$11.70	\$12.65	34 x 4 1/2	\$33.00	\$35.00
30 x 3 1/2	15.75	17.00	35 x 4 1/2	34.00	36.05
32 x 3 1/2	16.75	18.10	36 x 4 1/2	35.00	37.10
33 x 4	23.55	25.25	37 x 5	41.95	44.45
34 x 4	24.35	26.05	38 x 5 1/2	54.00	57.30

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